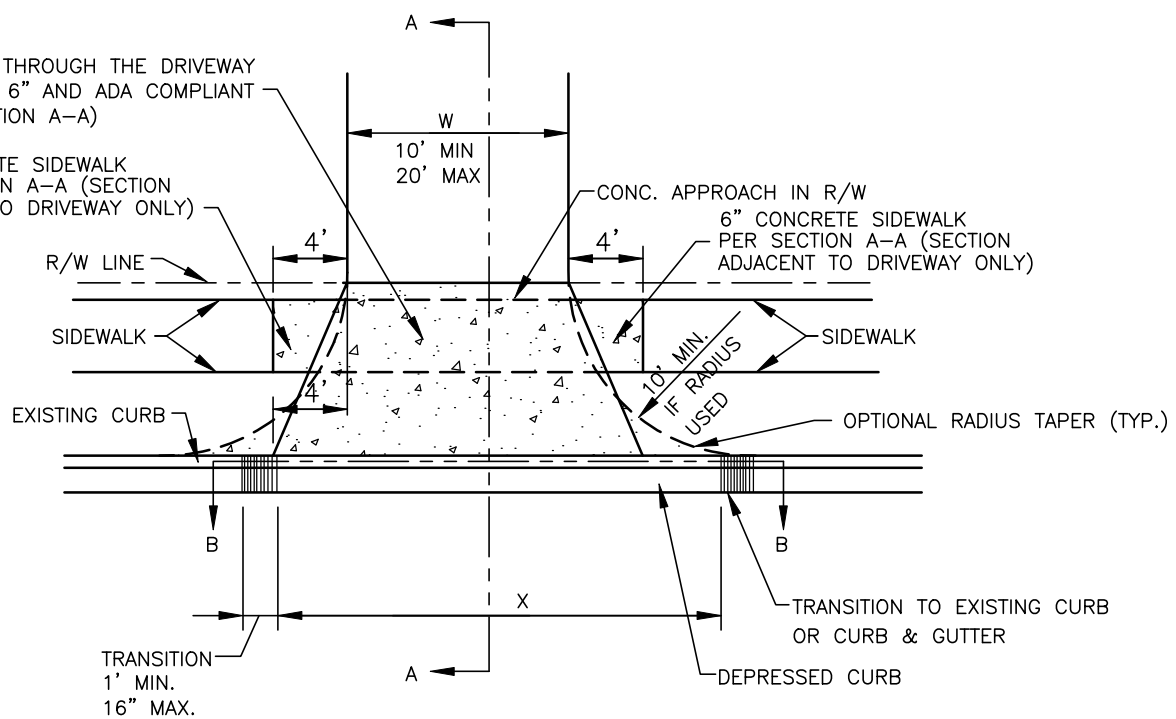
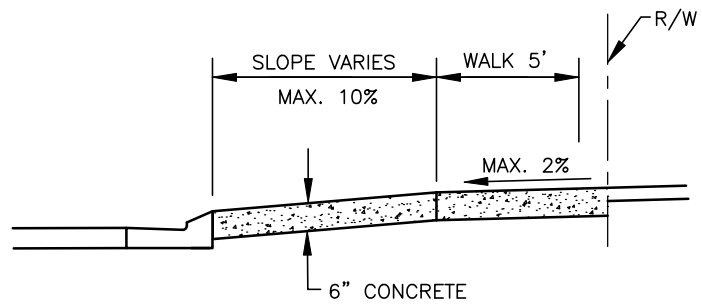


SIDEWALK THROUGH THE DRIVEWAY SHALL BE 6" AND ADA COMPLIANT (SEE SECTION A-A)

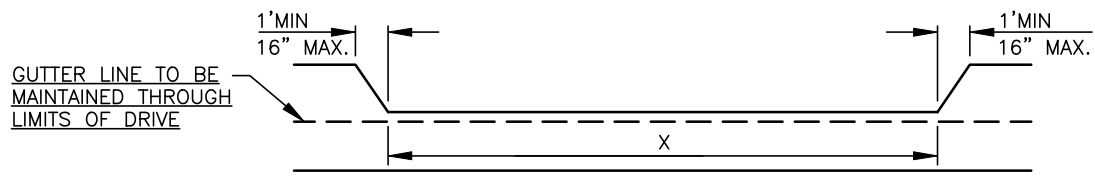
6" CONCRETE SIDEWALK PER SECTION A-A (SECTION ADJACENT TO DRIVEWAY ONLY)



PLAN



SECTION A-A



SECTION B-B

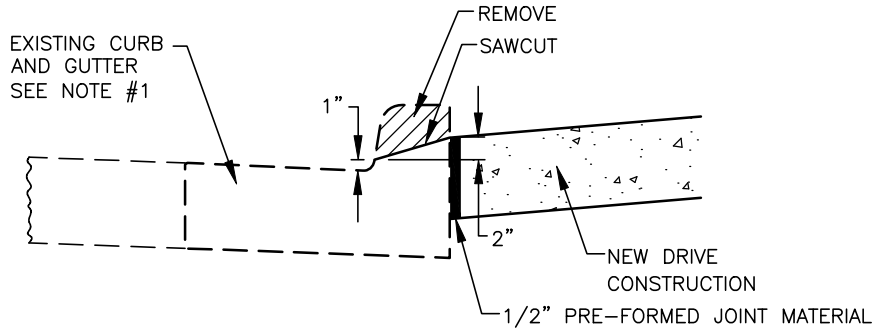
NOTES:

1. WHEN A NEW DRIVE IS CONSTRUCTED ON A STREET WITH EXISTING VERTICAL CURB, THE EXISTING CURB SHALL BE DEPRESSED IN ACCORDANCE WITH THE STANDARD DETAIL FOR EXISTING CURB ON SHEET II-2. EXISTING ROLLED CURB MAY REMAIN IN PLACE.
2. WHEN A DRIVE IS CONSTRUCTED WITHIN THE LIMITS OF NEW ROAD CONSTRUCTION, THE CURB SHALL BE DEPRESSED IN ACCORDANCE WITH THE STANDARD DETAIL FOR NEW CURB ON SHEET II-2. ROLLED CURB & GUTTER TYPE "A" DOES NOT REQUIRE DEPRESSION.
3. A DRIVEWAY PERMIT SHALL BE OBTAINED FROM CITY ENGINEERS OFFICE FOR ALL DRIVEWAY CONSTRUCTION.
4. CURB AND GUTTER SHALL BE POURED THROUGH THE APPROACH TO MAINTAIN GUTTER PAN.

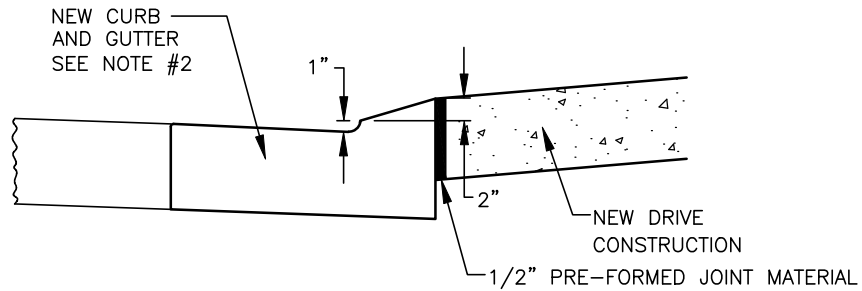
PREPARED BY: DLZ INDIANA, LLC

Brian M. Smith
2/1/2022

APPROVED/REVISED BY THE BOARD OF PUBLIC WORKS & SAFETY				CITY OF MISHAWAKA, INDIANA	
ITEM	REVISION	APPROVED DATE		ENGINEERING STANDARDS	
	EFFECTIVE	APRIL 2007	RESIDENTIAL DRIVEWAY APPROACH		SHT. NO. II-1
	REVISED	FEBRUARY 2008			
	REVISED	MARCH 2013			
	REVISED	FEBRUARY 2022			



EXISTING VERTICAL CURB DEPRESSED
FOR NEW DRIVE CONSTRUCTION



DEPRESSED CURB
FOR NEW DRIVE CONSTRUCTION

NOTES:

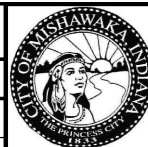
1. WHEN DEPRESSING EXISTING VERTICAL CURB, THE GUTTER PAN SHALL NOT BE REMOVED.
2. NEW VERTICAL CURB SHALL BE DEPRESSED FOR ALL DRIVES WITHIN THE LIMITS OF NEW ROAD CONSTRUCTION. THE DEPRESSED CURB SHALL BE CONSTRUCTED INTEGRAL WITH ADJACENT CURB IN ORDER TO MAINTAIN A CONSISTENT GUTTER LINE THROUGH THE DRIVE.

PREPARED BY: DLZ INDIANA, LLC



Brian M. Smith
2/1/2022

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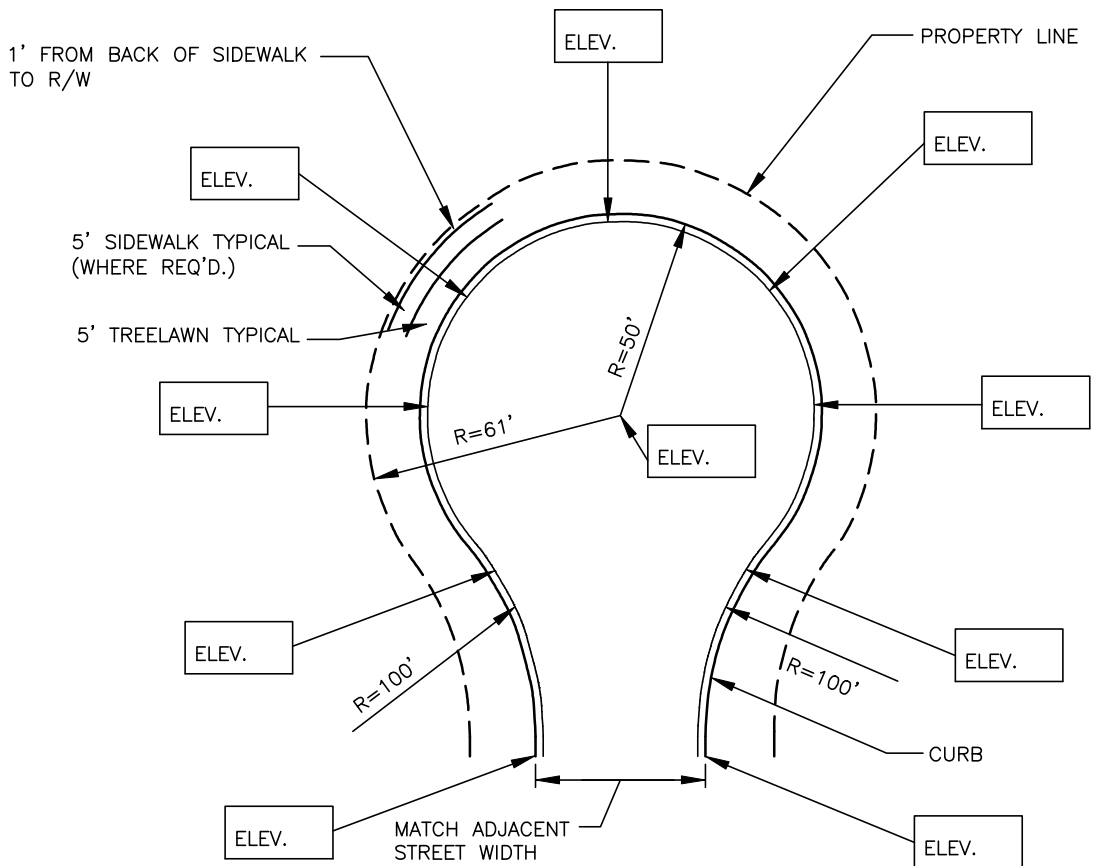
CITY OF MISHAWAKA, INDIANA

ENGINEERING STANDARDS

ITEM	REVISION	APPROVED DATE
	EFFECTIVE	APRIL 2007
	REVISED	FEBRUARY 2008
	REVISED	FEBRUARY 2022

DRIVE CONSTRUCTION
WITH DEPRESSED CURB

SHT. NO.
11-2



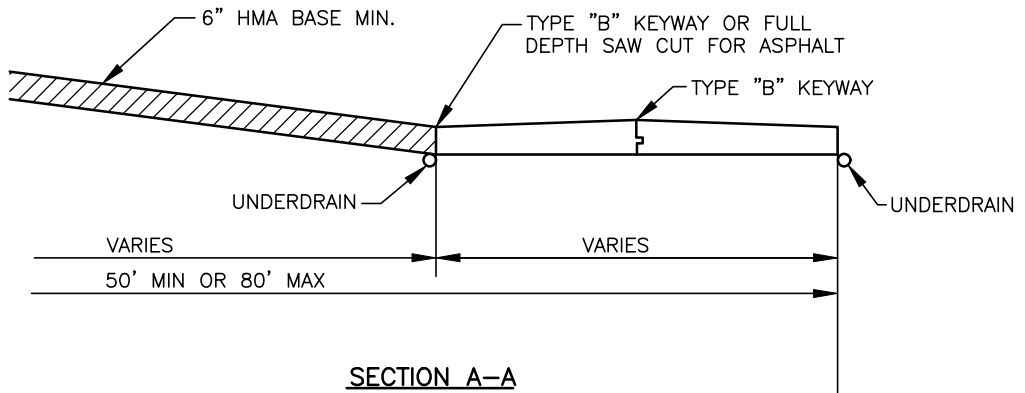
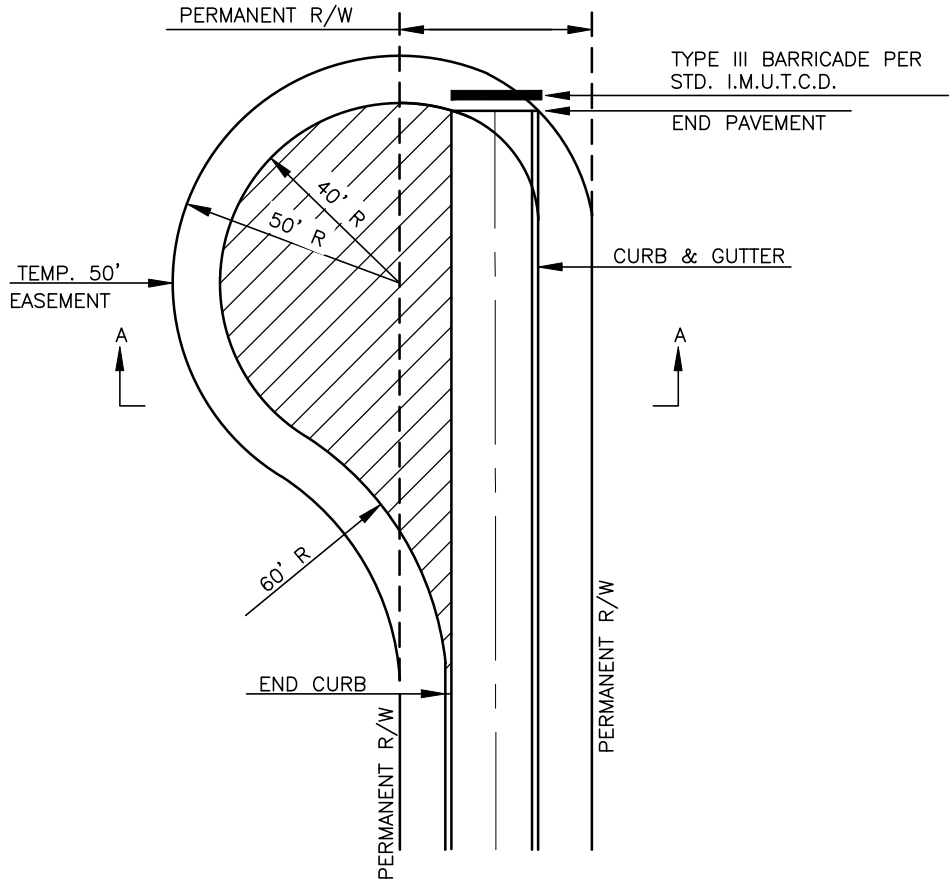
NOTES:

1. PAVEMENT SECTIONS AND STREETS WIDTHS (PAVEMENTS AND RIGHT OF WAY) SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS ON SHEET IV-1.
2. MAXIMUM CUL-DE-SAC STREET LENGTH ALLOWED IS 600 FEET, DEFINED TO THE CENTER OF THE CUL-DE-SAC.
3. OFFSET TYPE CUL-DE-SAC ALLOWED, WITH SAME MINIMUM DIMENSIONS.
4. CUL-DE-SACS WITH MINIMUM DIMENSIONS WILL NOT BE ALLOWED TO CONTAIN ISLANDS, LANDSCAPED, CURBED OR OTHERWISE.
5. TRAFFIC CONTROL SIGNING, AS NECESSARY, WILL BE IN ACCORDANCE WITH THE INDIANA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
6. PAVEMENT MATERIALS (CONCRETE OR ASPHALT) SHALL BE THE SAME AS THE REMAINDER OF THE STREET.
7. PARKING WILL NOT BE PERMITTED ON ANY CUL-DE-SAC.
8. WARNING SIGNS SHALL BE POSTED AT ENTRANCE TO STREET, INDICATING NO OUTLET.
9. DETAIL SHALL BE IDENTIFIED BY STREET NAME.
10. ELEVATIONS PROVIDED SHALL BE PROPOSED FLOW LINE OF GUTTER.
11. ONE DETAIL SHALL BE PROVIDED FOR EACH CUL-DE-SAC.
12. SCALE SHALL BE 1"=40' OR LARGER.

PREPARED BY: DLZ INDIANA, LLC

Brian M. Smith
2/1/2022

APPROVED/REVISED BY THE BOARD OF PUBLIC WORKS & SAFETY				CITY OF MISHAWAKA, INDIANA	
ITEM	REVISION	APPROVED DATE		ENGINEERING STANDARDS	
	EFFECTIVE	APRIL 2007	CUL-DE-SAC DETAIL		
	REVISED	FEBRUARY 2022			
			SHT. NO. 11-3		

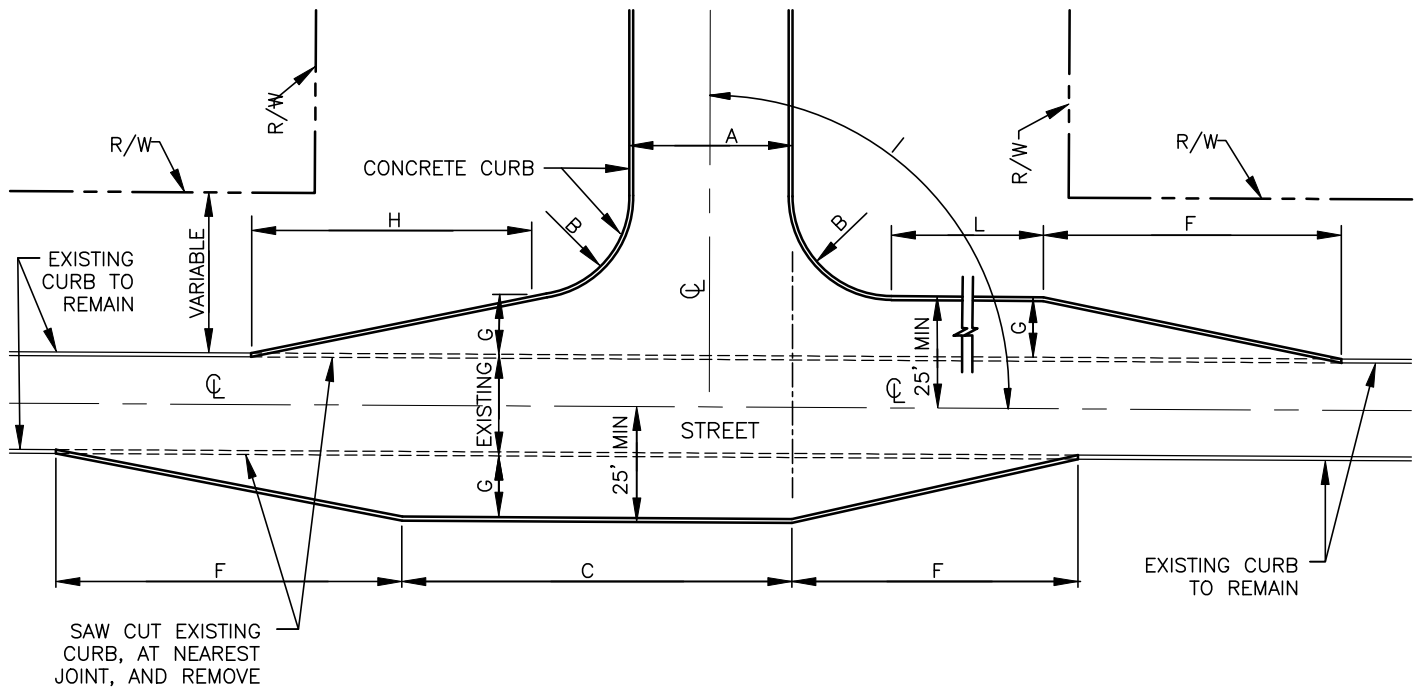


REQUIREMENTS:
 TEMPORARY CUL-DE-SAC SHALL BE REQUIRED ON DEAD END STREETS THAT WILL NOT BE EXTENDED TO A PERMANENT CUL-DE-SAC PRIOR TO NOVEMBER OF THE CURRENT YEAR.

PREPARED BY: DLZ INDIANA, LLC

Brian M. Smith
 2/1/2022

APPROVED/REVISED BY THE BOARD OF PUBLIC WORKS & SAFETY				CITY OF MISHAWAKA, INDIANA		
ITEM	REVISION	APPROVED DATE		ENGINEERING STANDARDS		
	EFFECTIVE	APRIL 2007	TEMPORARY CUL-DE-SAC DETAIL			SHT. NO. 11-4
	REVISED	FEBRUARY 2022				



LEGEND

- A= CITY ENGINEERING STANDARDS, SHEET IV-1
- B= 20' MINIMUM RADIUS
- C= 100' MINIMUM
- F= 1:10 TAPER-100' MINIMUM, SEE TABLE
- G= 12' MINIMUM
- H= 1:5 TAPER-50' MINIMUM, SEE TABLE
- I= 75° MINIMUM-105° MAXIMUM
- L= 150' MINIMUM

DESIGN SPEED	F + L
30	250 FEET
40	370 FEET
50	500 FEET

PUBLIC STREET APPROACH DETAIL

NOTES:

1. WHEN "I" IS OTHER THAN 90° "B" (EDGE RADII) SHALL BE APPROVED BY THE ENGINEER.
2. DECEL LANE AND PASSING BLISTER REQUIRED IF AADT IS GREATER THAN OR EQUAL TO 5,000 FOR TWO LANES OR GREATER THAN OR EQUAL TO 10,000 FOR FOUR LANES, OR AS DIRECTED BY ENGINEER.

PREPARED BY: DLZ INDIANA, LLC



Brian M. Smith
2/1/2022

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CITY OF MISHAWAKA, INDIANA

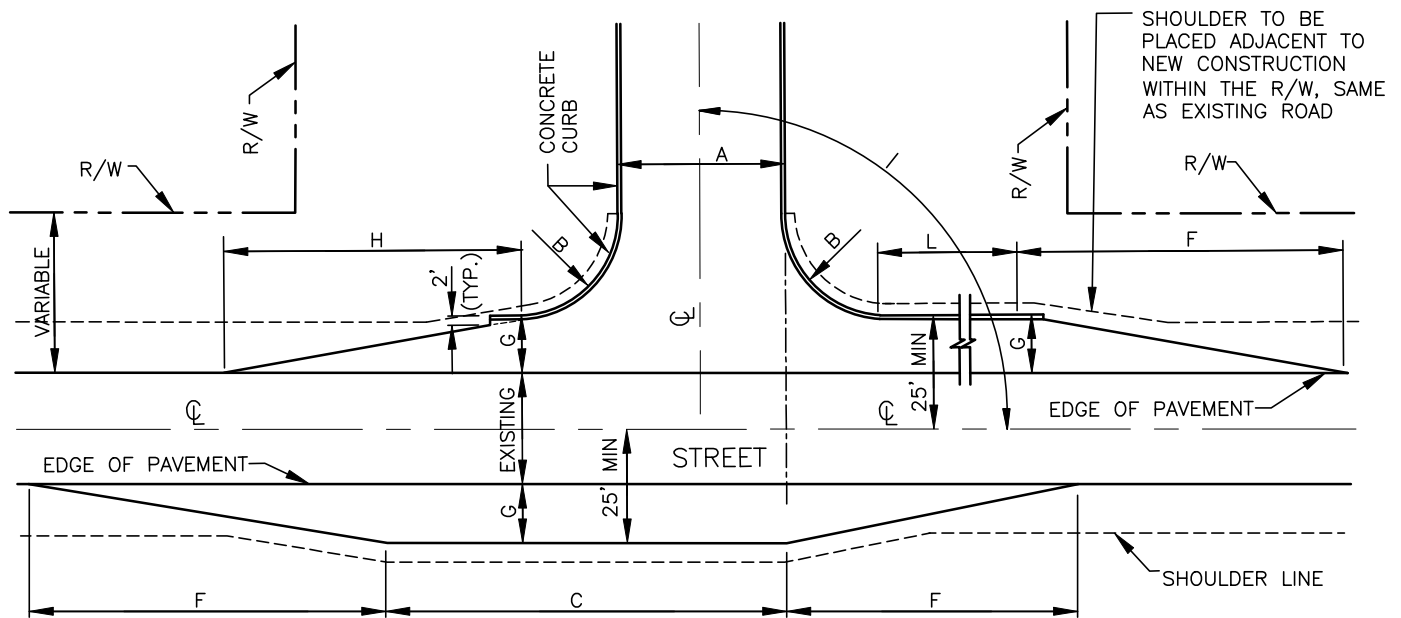
ENGINEERING STANDARDS

SHT. NO.

PUBLIC STREET APPROACH

11-5

ITEM	REVISION	APPROVED DATE
	EFFECTIVE	APRIL 2007
	REVISED	FEBRUARY 2022



LEGEND

- A= CITY ENGINEERING STANDARDS, SHEET IV-1
- B= 20' MINIMUM RADIUS
- C= 100' MINIMUM
- F= 1:10 TAPER-100' MINIMUM, SEE TABLE
- G= 12' MINIMUM
- H= 1:5 TAPER-50' MINIMUM, SEE TABLE
- I= 75° MINIMUM-105° MAXIMUM
- L= 150' MINIMUM

DESIGN SPEED	F + L
30	250 FEET
40	370 FEET
50	500 FEET

**PUBLIC STREET APPROACH DETAIL
WITH SHOULDERS**

NOTES:

1. WHEN "I" IS OTHER THAN 90° "B" (EDGE RADII) SHALL BE APPROVED BY THE ENGINEER.
2. DECEL LANE AND PASSING BLISTER REQUIRED IF AADT IS GREATER THAN OR EQUAL TO 5,000 FOR TWO LANES OR GREATER THAN OR EQUAL TO 10,000 FOR FOUR LANES, OR AS DIRECTED BY ENGINEER.

PREPARED BY: DLZ INDIANA, LLC



Brian M. Smith
2/1/2022

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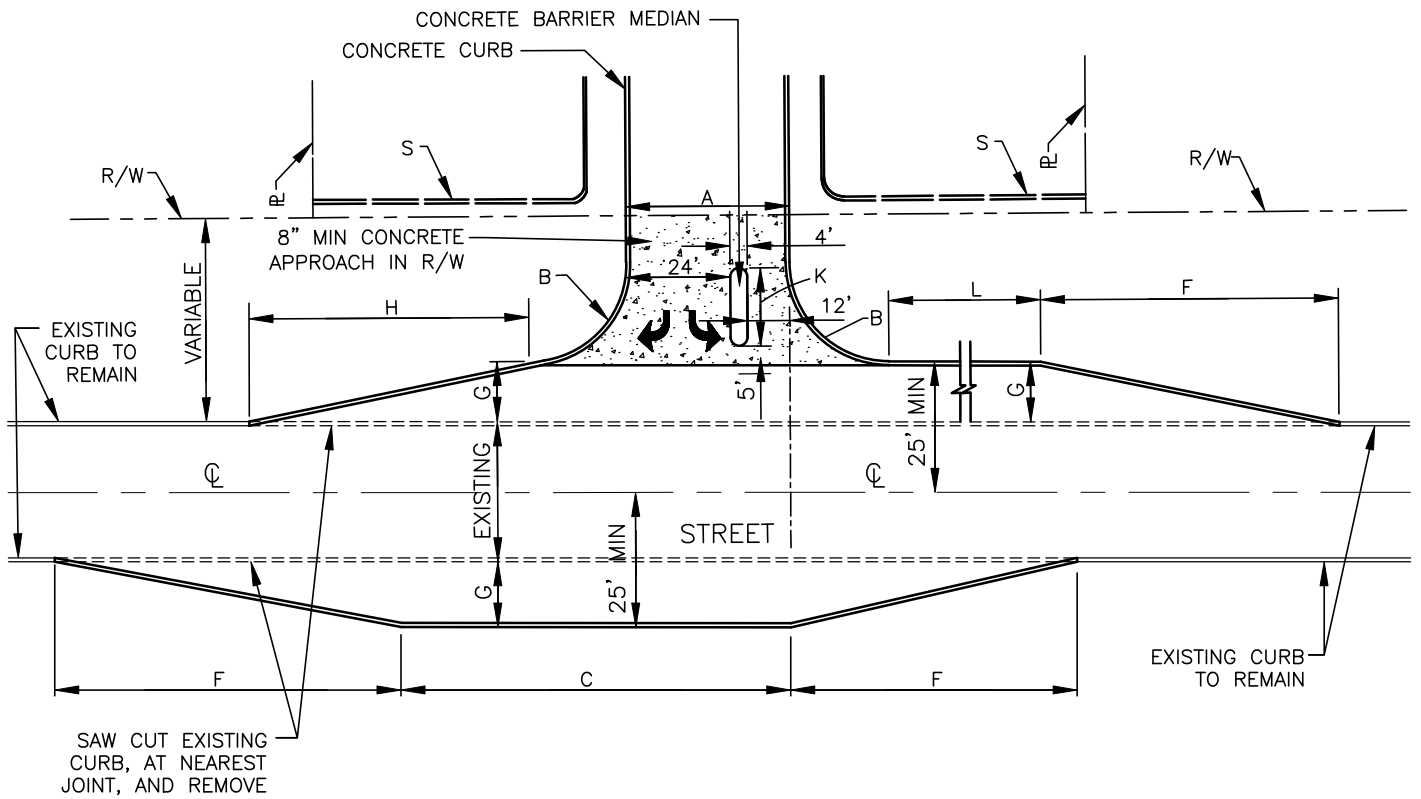
CITY OF MISHAWAKA, INDIANA

ITEM	REVISION	APPROVED DATE
	EFFECTIVE	APRIL 2007
	REVISED	FEBRUARY 2022

ENGINEERING STANDARDS

PUBLIC STREET APPROACH
WITH SHOULDERS

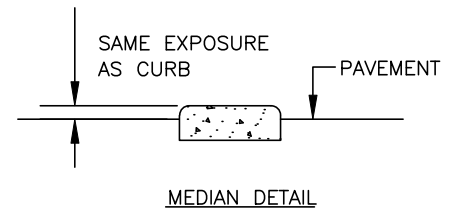
SHT. NO.
11-6



LEGEND

- A= 40'
- B= 20' MINIMUM RADIUS
- C= 100' MINIMUM
- F= 1:10 TAPER-100' MINIMUM, SEE TABLE
- G= 12' MINIMUM
- H= 1:5 TAPER-50' MINIMUM, SEE TABLE
- K= 20' MINIMUM BARRIER MEDIAN
- L= 150' MINIMUM
- S= BARRIERS

DESIGN SPEED	F + L
30	250 FEET
40	370 FEET
50	500 FEET



**COMMERCIAL DRIVE APPROACH DETAIL
TYPE I (MEDIAN)**

NOTES:

1. ALL DECEL LANES AND PASSING BLISTER PAVEMENTS SHALL MATCH EXISTING, ADJACENT PAVEMENT TYPE AND SECTION.
2. ALL APPROACHES SHALL BE CONCRETE PAVEMENT.
3. DECEL LANE AND PASSING BLISTER REQUIRED IF AADT IS GREATER THAN OR EQUAL TO 5,000 FOR TWO LANES OR GREATER THAN OR EQUAL TO 10,000 FOR FOUR LANES, OR AS DIRECTED BY ENGINEER.

PREPARED BY: DLZ INDIANA, LLC



Brian M. Smith
2/1/2022

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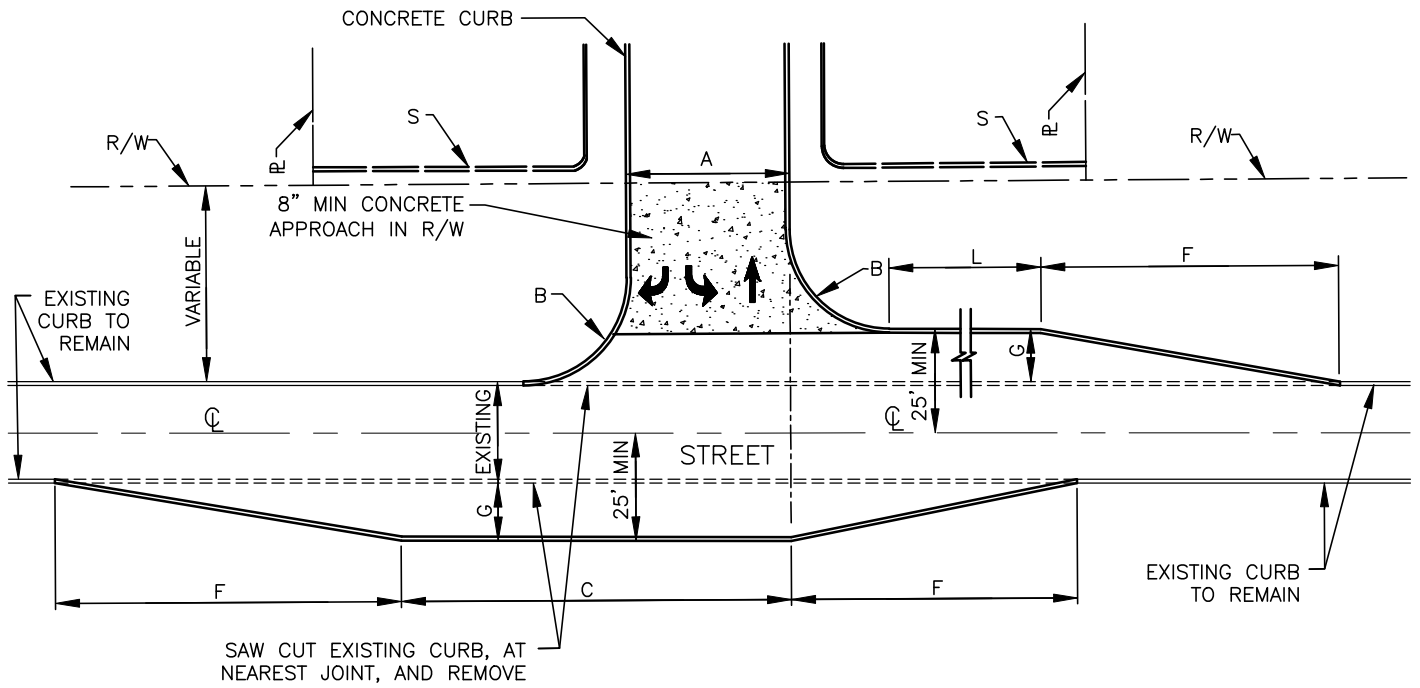
CITY OF MISHAWAKA, INDIANA

ITEM	REVISION	APPROVED DATE
	EFFECTIVE	APRIL 2007
	REVISED	FEBRUARY 2022

ENGINEERING STANDARDS

COMMERCIAL DRIVE
APPROACH TYPE I

SHT. NO.
11-7



LEGEND

- A= 38'
- B= 20' MINIMUM RADIUS
- C= 100' MINIMUM
- F= 1:10 TAPER-100' MINIMUM, SEE TABLE
- G= 12' MINIMUM
- L= 150' MINIMUM
- S= BARRIERS

DESIGN SPEED	F + L
30	250 FEET
40	370 FEET
50	500 FEET

**COMMERCIAL DRIVE APPROACH DETAIL
TYPE IA**

NOTES:

1. ALL DECEL LANES AND PASSING BLISTER PAVEMENTS SHALL MATCH EXISTING, ADJACENT PAVEMENT TYPE AND SECTION.
2. ALL APPROACHES SHALL BE CONCRETE PAVEMENT.
3. DECEL LANE AND PASSING BLISTER REQUIRED IF AADT IS GREATER THAN OR EQUAL TO 5,000 FOR TWO LANES OR GREATER THAN OR EQUAL TO 10,000 FOR FOUR LANES, OR AS DIRECTED BY ENGINEER

PREPARED BY: DLZ INDIANA, LLC



Brian M. Smith
2/1/2022

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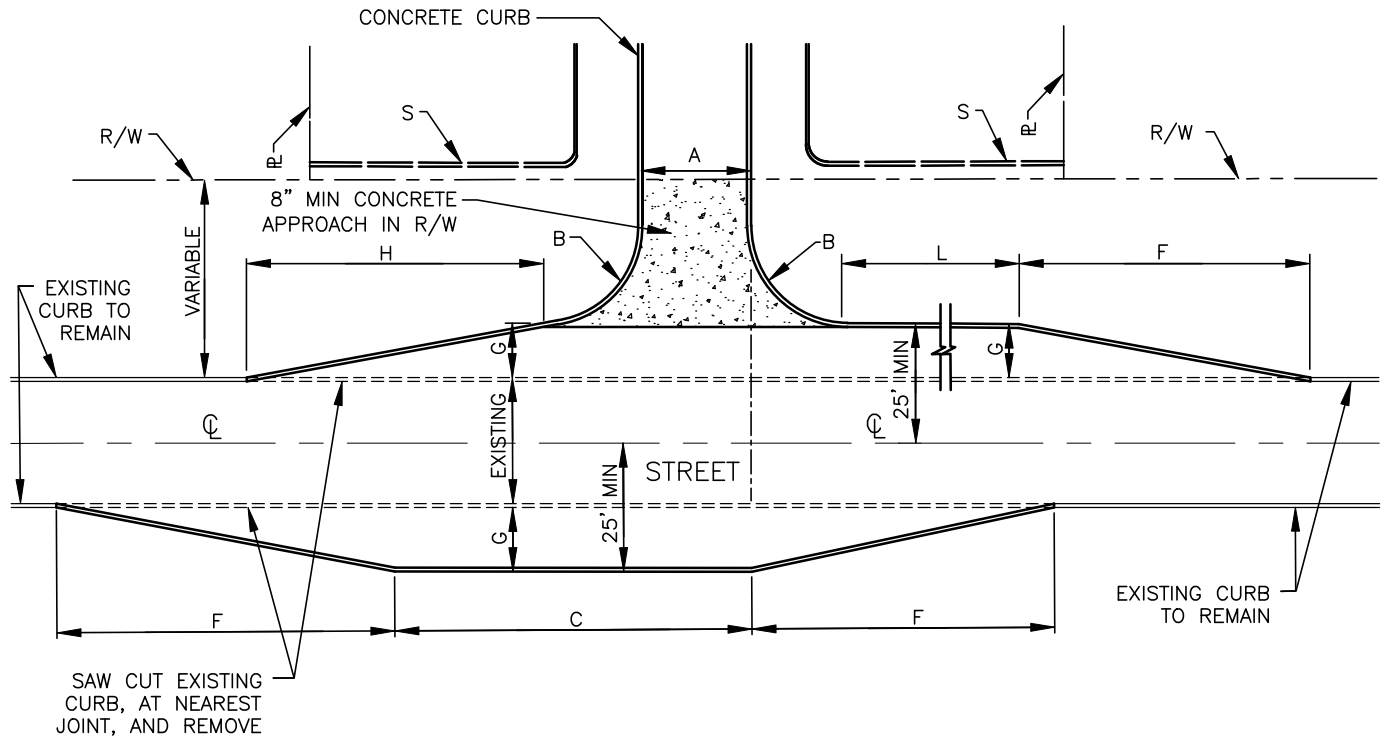
CITY OF MISHAWAKA, INDIANA

ENGINEERING STANDARDS

ITEM	REVISION	APPROVED DATE
	EFFECTIVE	APRIL 2007
	REVISED	FEBRUARY 2022

COMMERCIAL DRIVE
APPROACH TYPE IA

SHT. NO.
11-8



- LEGEND**
- A= 24'
 - B= 20' MINIMUM RADIUS
 - C= 100' MINIMUM
 - F= 1:10 TAPER-100' MINIMUM, SEE TABLE
 - G= 12' MINIMUM
 - H= 1:5 TAPER-50' MINIMUM, SEE TABLE
 - L= 150' MINIMUM
 - S= BARRIERS

DESIGN SPEED	F + L
30	250 FEET
40	370 FEET
50	500 FEET

**COMMERCIAL DRIVE APPROACH DETAIL
TYPE II**

NOTES:

1. ALL DECEL LANES AND PASSING BLISTER PAVEMENTS SHALL MATCH EXISTING, ADJACENT PAVEMENT TYPE AND SECTION.
2. ALL APPROACHES SHALL BE CONCRETE PAVEMENT.
3. DECEL LANE AND PASSING BLISTER REQUIRED IF AADT IS GREATER THAN OR EQUAL TO 5,000 FOR TWO LANES OR GREATER THAN OR EQUAL TO 10,000 FOR FOUR LANES, OR AS DIRECTED BY ENGINEER.

PREPARED BY: DLZ INDIANA, LLC



Brian M. Smith
2/1/2022

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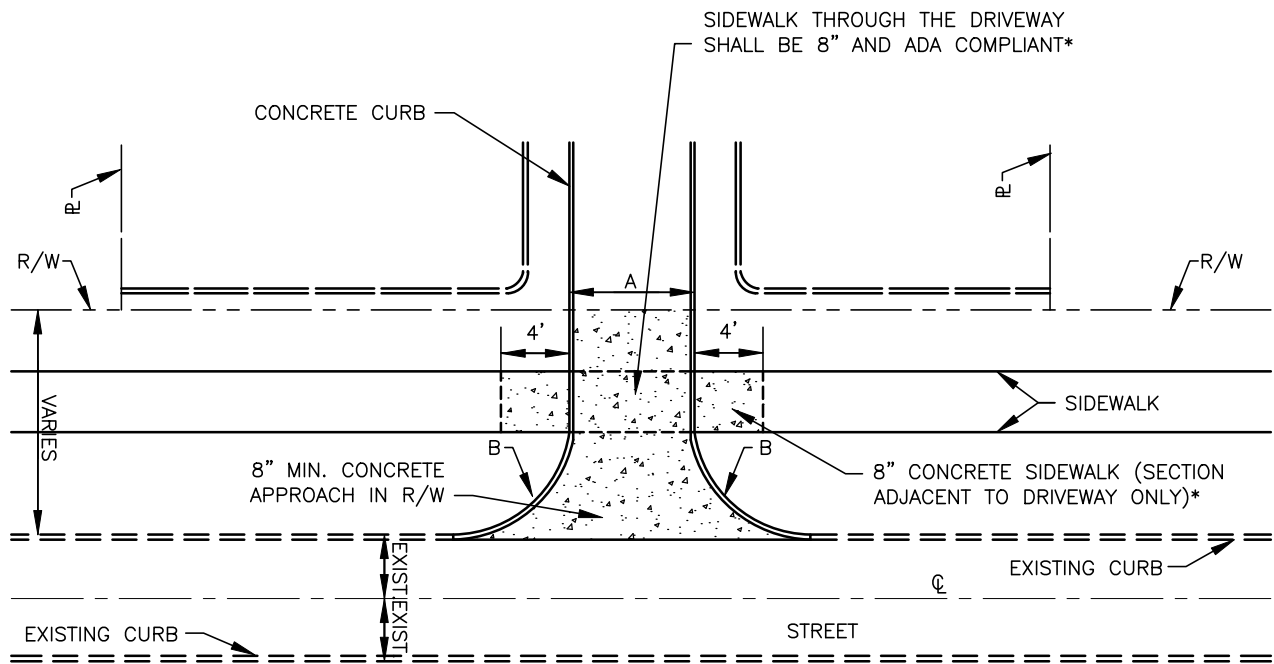
CITY OF MISHAWAKA, INDIANA

ENGINEERING STANDARDS

ITEM	REVISION	APPROVED DATE
	EFFECTIVE	APRIL 2007
	REVISED	FEBRUARY 2022

COMMERCIAL DRIVE
APPROACH TYPE II

SHT. NO.
11-9



LEGEND

A= 24'

B= 20' MINIMUM RADIUS

COMMERCIAL DRIVE APPROACH DETAIL
TYPE III

NOTES:

* SIDEWALK PLACEMENT THROUGH AND ADJACENT TO THE DRIVE IS REQUIRED IF THERE IS AN EXISTING OR PROPOSED SIDEWALK.

PREPARED BY: DLZ INDIANA, LLC



Brian M. Smith
2/1/2022

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CITY OF MISHAWAKA, INDIANA

ITEM	REVISION	APPROVED DATE
	EFFECTIVE	APRIL 2007
	REVISED	FEBRUARY 2022

ENGINEERING STANDARDS

COMMERCIAL DRIVE
APPROACH TYPE III

SHT. NO.

11-10